

GENERAL NOTES

1. BASIS OF BEARINGS IS: "THE EAST LINE OF THE NORTHEAST QUARTER OF SECTION 15, TOWNSHIP 12 SOUTH, RANGE 13 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, PIMA COUNTY, ARIZONA BEARING S00°01'14"E (ASSUMED BEARING)" ACCORDING TO A.L.T.A./N.S.P.S. AND TOPOGRAPHICAL SURVEYS PREPARED BY HARVEY LAND SURVEYING, INC. SEALED AND DATED FEBRUARY 25, 2022 BY COLIN D. HARVEY, RLS 42017 (HARVEY JN 2022-32)
2. BASIS OF ELEVATIONS IS: "A PIMA COUNTY OPUS CONTROL POINT #12S13E-J15, SAID POINT BEING IN SECTION 10, TOWNSHIP 12 SOUTH, RANGE 13 EAST, OF THE GILA AND SALT RIVER BASE AND MERIDIAN, PIMA COUNTY, ARIZONA. A #4 REBAR W/ TAG "RLS 13190" LOCATED APPROX. 64 FT NORTH OF LAMBERT LANE AND 91 FEET WEST OF LAMBERT PARK LANE. ELEVATION = 2604.32' (NAVD 88 DATUM)" PER AFOREMENTIONED SURVEY
3. BASIS OF ELEVATION, BASIS OF BEARINGS, BOUNDARIES, EASEMENTS, TOPOGRAPHIC DATA AND CULTURE SHOWN ARE FROM THE AFOREMENTIONED SURVEY PREPARED BY HARVEY LAND SURVEYING INC.
4. THIS PROJECT IS WITHIN THE CANADA CROSSROADS DEVELOPMENT. REFER TO DEVELOPMENT PLAN OV12-98-12.
5. GROSS AREA OF DEVELOPMENT IS 33,103.50 SF OR 0.76 ACRES, MORE OR LESS.
6. GROSS FLOOR AREA (GFA) IS 1,649 SF.
7. MAX FLOOR AREA RATIO (FAR) IS 0.25 PER OVZCR TABLE 23-2B. FAR PROPOSED IS 1,649/ 33,103.50 = 0.05
8. TOTAL MILES OF NEW PUBLIC STREETS IS 0.
9. TOTAL MILES OF NEW PRIVATE STREETS IS 0.
10. OV10-97-9 IS APPLICABLE TO THE PROJECT AND DOCUMENTS APPROVAL OF A BOARD OF ADJUSTMENT INCREASE IN MAXIMUM BUILDING HEIGHT FROM 18' TO 25'.
11. ASSURANCES FOR WATER SERVICE, SITE STABILIZATION AND LANDSCAPING MUST BE POSTED PRIOR TO ISSUANCE OF GRADING PERMITS
12. SOLID WASTE REFUSE WILL BE STORED IN SHARED EXISTING DUMPSTER ENCLOSURES AT THE SOUTHEAST CORNER OF CANADA CROSSROADS CENTER.

PLANNING GENERAL NOTES

1. THE USE IS CONVENIENCE USE: DRIVE-THRU RESTAURANT. APPROVAL OF A CONDITIONAL USE PERMIT IN ACCORDANCE WITH OVZCR 22.5 IS REQUIRED PER OVZCR 23.3 TABLE 23-1
2. THIS PROJECT REQUIRES TOWN COUNCIL APPROVAL OF A REDUCTION TO THE REQUIRED 250-FOOT SEPARATION BETWEEN CONVENIENCE USES AND RESIDENTIALLY-USED PROPERTIES, AS REQUIRED BY OVZCR 25.1.B.6.a. THE PROPOSED SEPARATION IS 195 FEET.
3. THE MAXIMUM BUILDING HEIGHT IS 25' PER BOARD OF ADJUSTMENT INCREASE DOCUMENTED IN OV10-97-9. THE PROPOSED BUILDING HEIGHT IS 20'.
4. NO OVERLAY ZONES ARE APPLICABLE.
5. THIS PROJECT IS NOT INCLUDED IN THE GENERAL PLAN SIGNIFICANT RESOURCE AREAS (SRA).
6. THIS PROJECT IS NOT SUBJECT TO THE REQUIREMENTS FOR ENVIRONMENTALLY SENSITIVE LANDS (ESL).
7. MINIMUM OPEN SPACE REQUIREMENT IS 25% OF LOT AREA=8,276 SF PER OVZCR TABLE 23-2B. TOTAL PROVIDED IS 8,279 SF.
8. MINIMUM LANDSCAPED COMMON AREA REQUIRED IS 2% OF LOT AREA=662 SF. TOTAL PROVIDED IS 886 SF.
9. NO LANDSCAPE BUFFER YARDS ALONG INTERIOR BORDERS ARE REQUIRED PER OVZCR TABLE 27.7. A MINIMUM 30' BUFFER YARD 'B' IS REQUIRED ALONG THE STREET FRONTAGE PER OVZCR TABLE 27-9. A STREET BUFFER YARD WITH 20' ONSITE AND 10' IN THE PUBLIC RIGHT-OF-WAY EXISTS PER APPROVED DEVELOPMENT PLAN BK19, PG30.
10. NO SCREENING ALONG INTERIOR BORDERS IS REQUIRED PER OVZCR TABLE 27-12.
11. SETBACKS REQUIRED:
FRONT: 20' (76.7' PROVIDED). ALSO 105' FROM SECTION LINE PER DOCKET 7397 AT PAGE 727 (EQUATES TO 30' FROM FRONT PROPERTY LINE)
PERIMETER SIDE & REAR SETBACKS FOR CANADA CROSSROADS DO NOT IMPACT THE SUBJECT PARCEL.
12. EXISTING ZONING IS C-N
13. ALL SIGNAGE AND LIGHTING TO BE ADDRESSED AS PART OF A SEPARATE REVIEW AND APPROVAL PROCESS
14. ALL PUBLIC ART REQUIREMENTS FOR CANADA CROSSROADS HAVE BEEN MET.

ENGINEERING GENERAL NOTES

1. THE DESIGN VEHICLE IS SU-30 (EXCLUDING DRIVE-THRU). DESIGN SPEED IS 5 MPH
2. ALL NEW PUBLIC ROADS WITHIN AND ADJACENT TO THIS PROJECT WILL BE CONSTRUCTED IN ACCORDANCE WITH APPROVED PLANS. SEPARATE PUBLIC IMPROVEMENT AND CONSTRUCTION PLANS WILL BE SUBMITTED TO THE TOWN ENGINEER'S OFFICE FOR REVIEW AND APPROVAL
3. ANY RELOCATION OR MODIFICATION OF EXISTING UTILITIES AND/OR PUBLIC IMPROVEMENTS NECESSITATED BY THE PROPOSED DEVELOPMENT WILL BE AT NO EXPENSE TO THE PUBLIC
4. THIS PROJECT DOES NOT PROPOSE ANY SUBSTANTIAL CHANGE TO THE DRAINAGE SCHEME OR STORMWATER DETENTION REQUIREMENTS APPROVED FOR THE CANADA CROSSROADS DEVELOPMENT PLAN OV12-98-12. THE PROJECT PROPOSES 73% IMPERVIOUS AREAS COMPARED TO 88% IMPERVIOUS AREAS ON THE APPROVED DEVELOPMENT PLAN OV12-098-12.

ORO VALLEY WATER UTILITY GENERAL NOTES

1. THIS DEVELOPMENT MUST COMPLY WITH THE ORO VALLEY WATER UTILITY SPECIFICATIONS MANUAL DURING ALL PHASES OF CONSTRUCTION
2. THIS PROJECT WILL BE SERVED BY ORO VALLEY WATER UTILITY WHICH HAS BEEN DESIGNATED AS HAVING AN ASSURED 100 YEAR WATER SUPPLY BY THE DIRECTOR OF WATER RESOURCES. ANY AND ALL WELLS MUST BE ABANDONED PER ADWR REGULATIONS
3. A LINE EXTENSION AGREEMENT MUST BE IN PLACE PRIOR TO ANY WORK ON THE WATER INFRASTRUCTURE FOR THIS PROJECT BEGINS.
4. ALL METERS SHALL HAVE A BACKFLOW PROTECTION DEVICE INSTALLED ON THE CUSTOMER SIDE OF THE METER
5. ALL FIRE SERVICES SHALL HAVE A BACKFLOW PROTECTION DEVICE INSTALLED ON THEM
6. ORO VALLEY WATER UTILITY WILL BE THE WATER SERVICE PROVIDER.

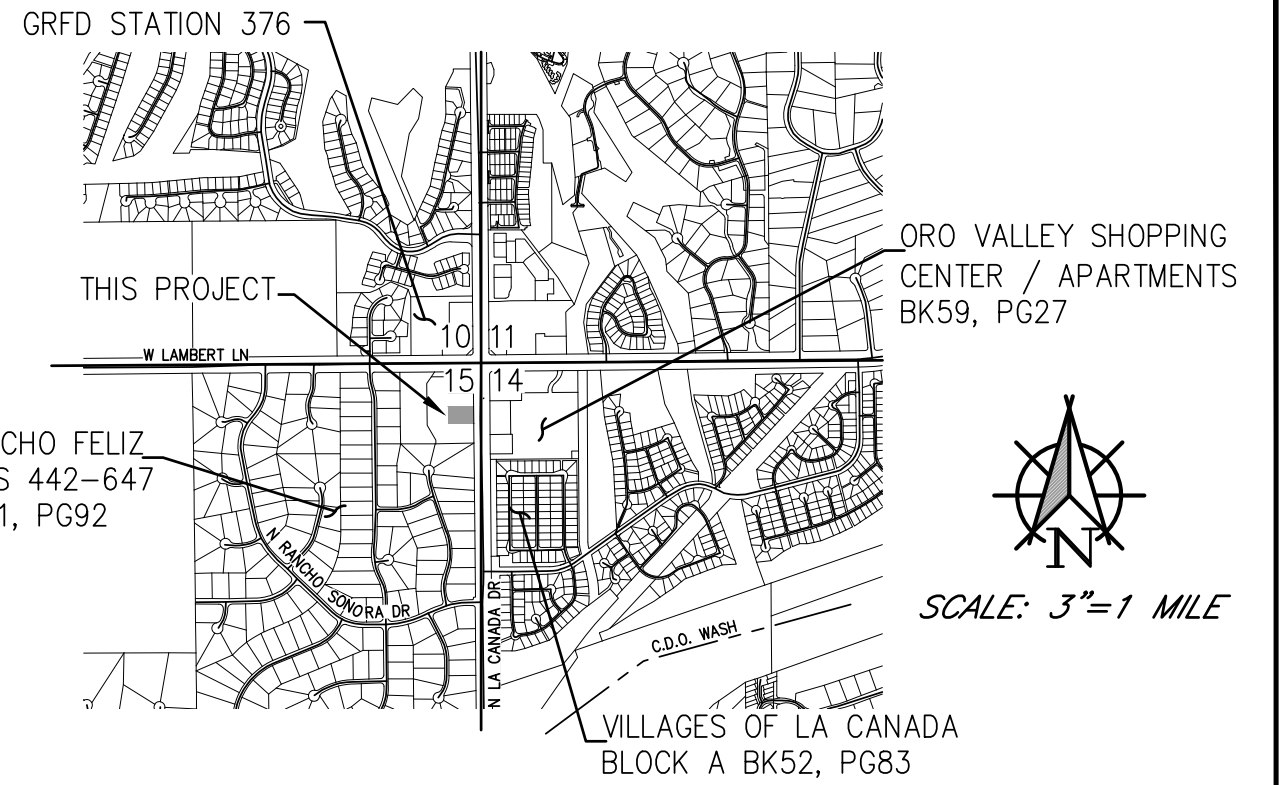
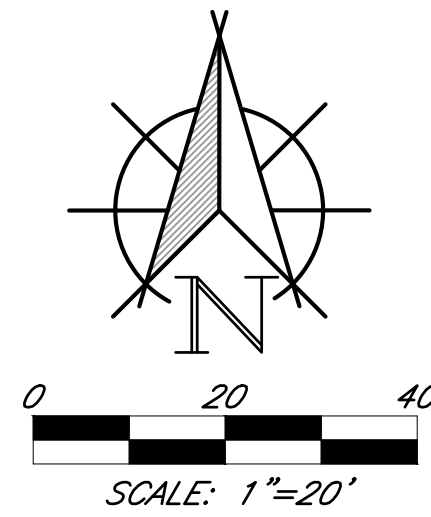
GOLDER RANCH FIRE GENERAL NOTES

1. FIRE HYDRANTS CONNECTED TO AN APPROVED WATER SUPPLY OF 1500 GPM FOR FIRE PROTECTION MUST BE INSTALLED AND IN SERVICE PRIOR TO COMBUSTIBLE MATERIAL DELIVERY TO THE SITE. TEMPORARY CONSTRUCTION OFFICE TRAILERS ARE CONSIDERED COMBUSTIBLE MATERIAL
2. APPROVED FIRE APPARATUS ACCESS ROADS MUST BE INSTALLED AND IN SERVICE PRIOR TO COMBUSTIBLE MATERIAL DELIVERY TO THE SITE
3. AUTOMATIC FIRE SPRINKLER SYSTEMS ARE REQUIRED IN ALL NEWLY CONSTRUCTED COMMERCIAL BUILDINGS
4. TEMPORARY STREET SIGNS MUST BE INSTALLED AT EACH STREET INTERSECTION WHEN CONSTRUCTION OF NEW ROADWAYS ALLOWS PASSAGE OF VEHICLES. ALL STRUCTURES UNDER CONSTRUCTION MUST BE CLEARLY IDENTIFIED WITH AN APPROVED ADDRESS
5. THE INSTALLATION OF TRAFFIC CONTROL SIGNALING DEVICES AND/OR ELECTRICALLY OPERATED GATES ON FIRE APPARATUS ACCESS ROADS SHALL INCLUDE PREEMPTIVE CONTROL EQUIPMENT COMPATIBLE WITH THE FIRE DEPARTMENT'S EXISTING SYSTEM

BUILDING CODES

1. THE FOLLOWING CODES AND STANDARDS SHALL BE APPLICABLE TO THIS DEVELOPMENT:
INTERNATIONAL CODES WITH LOCAL AMENDMENTS
NATIONAL ELECTRICAL CODE
ADA STANDARDS FOR ACCESSIBLE DESIGN
GOLDER RANCH FIRE DISTRICT STANDARDS AND FORMS
TOWN OF ORO VALLEY POOL CODE
PAG STANDARD SPECIFICATIONS & DETAILS FOR PUBLIC IMPROVEMENTS
TOWN OF ORO VALLEY DRAINAGE CRITERIA MANUAL
TOWN OF ORO VALLEY SUBDIVISION STREET STANDARDS AND POLICIES MANUAL
TOWN OF ORO VALLEY ZONING CODE, CURRENT REVISED
ORO VALLEY TOWN CODE, CURRENT REVISED

CONCEPTUAL SITE PLAN TROPICAL SMOOTHIE - LA CANADA DR 2202232



LOCATION MAP

LOCATED IN THE N.E. 1/4 OF SEC 15, T12S, R13E, G&SRM, TOWN OF ORO VALLEY, PIMA COUNTY, ARIZONA

LEGEND

- SUBJECT PARCEL BOUNDARY
- ADJACENT PROPERTY/ROW LINE
- EASEMENT LINE
- CENTERLINE
- EXISTING CURB AND EDGE OF ASPHALT PAVEMENT
- EXISTING 5' CONTOUR LINE
- EXISTING 1' CONTOUR LINE
- EXISTING WATERLINE, VALVE, REDUCER & METER
- EXISTING UNDERGROUND ELECTRIC
- EXISTING GAS LINE
- EXISTING SEWER, FLOW DIRECTION & MANHOLE
- PROPOSED FIRE SERVICE LINE, REDUCER, HORIZONTAL BEND
- PROPOSED BUILDING CONNECTION SEWER & CLEANOUT
- PAVEMENT SAWCUT LINE
- VERT. CURB & DRAINAGE CURB OPENING
- EXIST LIGHT POLE
- SURVEY MONUMENT OR PIN AS NOTED
- FINAL GRADE (ADD 2500') W/ ABBREVIATED DESCRIPTION
- APPROXIMATE EXIST GRADE / MATCH-IN GRADE
- TOP OF CURB/SDWK, GUTTER, GRADE-BREAK, HIGH POINT, LOW POINT, INVERT, FINISHED FLOOR ELEVATION
- FLOW DIRECTION ARROW
- 100-YR ONSITE RUNOFF RATE
- ASPHALT PAVING
- CONCRETE/SIDEWALK
- BRICK-PATTERN STAMPED CONC. CROSSWALK
- PROPOSED FENCE
- APPROX 100-YR DETENTION BASIN PONDING LIMIT

CIVIL ENGINEER

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ARCHITECT

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SCOTTSDALE, AZ 85254
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OWNER/DEVELOPER

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SITE ADDRESS

10335 N LA CANADA DR
ORO VALLEY, AZ 85737

REF. NO: OV12-98-12
OV10-97-9
OV9-96-7
PRE-APP 2102451
CUP 2202252



CONCEPTUAL SITE PLAN TROPICAL SMOOTHIE-LA CANADA DR

LOCATED IN THE N.E. 1/4 OF SEC 15, T12S, R13E, G&SRM, TOWN OF ORO VALLEY, PIMA COUNTY, ARIZONA

CASE NO:	2202232	
JOB NO. D21017	SCALE HORIZ: 1"=20'	
DATE: JULY, 2023	CONTOUR INTERVAL=N/A	SHEET 1 OF 5

LOT	PROPOSED USE	SQUARE FOOTAGE	PARKING RATIO	TOTAL PARKING		TOTAL HANDICAP PARKING		LOADING ZONE RATIO	TOTAL LOADING ZONES		BIKE PARKING CLASS I		BIKE PARKING CLASS II	
				REQUIRED	PROVIDED	REQUIRED	PROVIDED		REQUIRED	PROVIDED	REQUIRED	PROVIDED	REQUIRED	PROVIDED
PAD 2	CONVENIENCE USE: DRIVE-THRU	1,649 SF RESTAURANT	9/1000	15	32(26 EXIST)	2	2	NONE REQ'd	0	0	0	0	2	2
OV12-98-12 REFERENCE INFO		5,950 SF RETAIL	5.5/1000 (CALCULATED)	33					1	1				
TOTALS				15	32	2	2		0	0	0	0	2	2

SHEET INDEX

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- 5 - GRADING DETAILS



NO.	BY	DATE	DESCRIPTION	APPROVED BY	DATE

GRADING NOTES (TOWN OF ORO VALLEY STANDARD NOTES)

- ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE W/ PAG STANDARD SPECIFICATIONS AND DETAILS FOR PUBLIC IMPROVEMENTS (PAG SSPI), EDITION OF 2015, EXCEPT AS MODIFIED HEREBY.
- ALL CONSTRUCTION AND TESTING METHODS SHALL BE IN CONFORMANCE WITH PAG SSPI, EDITION OF 2015 EXCEPT AS MODIFIED HEREBY.
- ALL WORK SHALL BE IN CONFORMANCE TO GRADING STANDARDS, CHAPTER 27: SECTION 27.9 OF THE ORO VALLEY ZONING CODE REVISED JULY 2007.
- EXCAVATION AND BACKFILL FOR STRUCTURES SHALL CONFORM TO PAG SSPI, SECTION 203-5.
- ALL CONCRETE SHALL CONFORM TO PAG SSPI SECTION 1006, CLASS S, 3,000 PSI COMPRESSIVE STRENGTH AT 28 DAYS, UNLESS OTHERWISE SPECIFIED.
- A STAMPED SET OF APPROVED PLANS SHALL BE KEPT IN AN EASILY ACCESSIBLE LOCATION ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION
- CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REGULATIONS.
- CONTRACTOR SHALL CALL BLUE STAKE (ARIZONA 811) TO VERIFY LOCATION OF ALL UTILITIES PRIOR TO COMMENCEMENT OF CONSTRUCTION
- CONTRACTORS SHALL OBTAIN ALL PERMITS REQUIRED BY GOVERNMENTAL AGENCIES.
- UPON COMMENCEMENT OF WORK, TRAFFIC CONTROL DEVICES SHALL BE POSTED AND MAINTAINED BY THE CONTRACTOR UNTIL SUCH TIME AS THE WORK IS COMPLETED. ALL WARNING SIGNS, BARRICADES, ETC. SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC DEVICES ADOPTED BY THE STATE OF ARIZONA PURSUANT TO A.R.S. 28-650.
- IF UNANTICIPATED CONDITIONS ARE ENCOUNTERED DURING THE COURSE OF CONSTRUCTION AND ARE BEYOND THE SCOPE OF DESIGN, THE ENGINEER SHALL SUBMIT THE NECESSARY REVISED OR SUPPLEMENTAL IMPROVEMENT PLANS FOR REVIEW AND APPROVAL BY THE TOWN OF ORO VALLEY PRIOR TO SUCH REVISIONS/CHANGES BEING MADE IN THE FIELD.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CARE AND MAINTENANCE OF EXISTING IMPROVEMENTS AND VEGETATION IN THE WORK AREA. PAVEMENT, CURBS, CART PATHS AND ANY OTHER OBSTRUCTIONS DAMAGED DURING CONSTRUCTION ARE TO BE REPLACED BY THE CONTRACTOR. ANY UNDERGROUND PIPES, IRRIGATION LINES, IRRIGATION CONTROLS, DRAINS, STRUCTURES, OR OBSTRUCTIONS NOT SHOWN ON THESE PLANS SHALL BE MOVED, ALTERED, OR REPAIRED BY THE CONTRACTOR WHEN ENCOUNTERED, AS DIRECTED BY THE ENGINEER, AND IS A DEFINITE PART OF THIS PROJECT.
- ACCEPTANCE OF THESE PLANS DOES NOT CONSTITUTE OR IMPLY ACCEPTANCE OF ANY OF THE FOLLOWING:
 - WALL(S), RETAINING OR OTHER TYPE(S).
 - ANY REINFORCED CONCRETE STRUCTURE(S)
 - ANY EMBANKMENT(S) WHOSE PRIMARY PURPOSE IS TO FUNCTION AS A RETENTION/DETENTION STRUCTURETHE ITEMS LISTED ABOVE ARE APPROVED FOR LOCATION ONLY. SEPARATE PLAN CHECKS AND PERMITS ARE REQUIRED, ALL IN ACCORDANCE WITH THE APPLICABLE CODES OF THE TOWN OF ORO VALLEY.
- THE CONTRACTOR SHALL GIVE FORTY-EIGHT (48) HOURS NOTICE WHEN HE SHALL REQUIRE THE SERVICES OF THE SURVEYOR OR ANY OTHER PERSON PROPERLY AUTHORIZED FOR SUCH PURPOSE AS LAYING OUT ANY PORTION OF THE WORK. HE SHALL ALSO DIG ALL STAKE HOLES NECESSARY TO GIVE LINE AND LEVELS, AND SHALL PROVIDE ASSISTANCE CALLED FOR BY THE SURVEYOR OR HIS ASSISTANTS UPON ANY PART OF THE WORK WHENEVER SO REQUESTED, AND SHALL PRESERVE ALL STAKES SET FOR THE LINES, LEVELS OR MEASUREMENTS OF THE WORK IN THEIR PROPER PLACES UNTIL AUTHORIZED TO REMOVE THEM BY THE ENGINEERS. ANY EXPENSE INCURRED IN REPLACING ANY STAKES WHICH THE CONTRACTOR OR HIS SUBORDINATES MAY HAVE FAILED TO PRESERVE SHALL BE CHARGED TO THE CONTRACTOR.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FURNISH, HAUL, AND APPLY ALL WATER REQUIRED FOR COMPACTION AND FOR THE CONTROL OF DUST FROM CONSTRUCTION ACTIVITY. THE COST THEREOF IS TO BE INCLUDED THE GRADING CONSTRUCTION PRICE.
- BUILDING SITES SHALL BE CONSTRUCTED WITHIN 0.10 FOOT OF FINISH BUILDING PAD ELEVATIONS AS STAKED BY THE SURVEYOR. STREETS AND PARKING AREAS SHALL BE CONSTRUCTED TO WITHIN +0.10 FEET OF FINISH SUBGRADE AS STAKED BY THE SURVEYOR.
- A REPORT OF GEOTECHNICAL ENGINEERING EVALUATION, INCLUDING RECOMMENDATIONS FOR GRADING PROCEDURES HAS BEEN PREPARED BY PATTISON ENGINEERING (DATED FEB. 9, 2022, PROJECT NO. 22-004) FOR THIS PROJECT. EARTHWORK SHALL CONFORM TO THE RECOMMENDATIONS CONTAINED IN SAID REPORT AND ANY AMENDMENTS THERETO.
- THE SOILS ENGINEER SHALL OBSERVE, INSPECT AND TEST ALL CONSTRUCTION OPERATIONS, INCLUDING BUT NOT LIMITED TO: CLEARING, GRUBBING, SUBGRADE PREPARATION, STRUCTURAL, TRENCH EXCAVATION AND BACKFILL, MATERIAL TESTING, TOGETHER WITH PLACEMENT OF FILL. SAID ENGINEER SHALL CERTIFY IN WRITING THAT ALL SOILS OPERATIONS AND MATERIALS USED FOR THIS DEVELOPMENT WERE PERFORMED IN ACCORDANCE WITH THE RECOMMENDATIONS AS SET FORTH IN THE GEOTECHNICAL INVESTIGATION OF RECORD AND ARE IN CONFORMANCE WITH THE ACCEPTED PLANS AND SPECIFICATIONS. CERTIFICATION, IN WRITING, ARE TO BE RECEIVED BY THE TOWN OF ORO VALLEY PRIOR TO THE REQUEST FOR FINAL INSPECTION AND RELEASE OF ASSURANCES.
- GRADING BOUNDARIES SHALL BE CLEARLY MARKED, AND ALL WORK WILL BE CONFINED TO APPROVED PROJECT LIMITS AS SHOWN ON THESE PLANS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND/OR THE SURVEYOR PROVIDING THE CONSTRUCTION LAYOUT TO VERIFY THE BENCHMARK AND COMPARE THE SITE CONDITIONS WITH THE PLANS AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OBSERVED. SHOULD ANY BENCHMARK, GRADE OR DESIGN INDICATED ON THE PLANS BE SUSPECT, THE ENGINEER SHALL BE NOTIFIED OF SAID BENCHMARK, GRADE OR DESIGN PROBLEM AT LEAST TWENTY-FOUR HOURS BEFORE CONSTRUCTION IS SCHEDULED TO BEGIN ON THE AFFECTED AREA.
- IT SHALL BE THE SOLE RESPONSIBILITY OF THE OWNER(S), THEIR SUCCESSORS OR ASSIGNS (AND/OR THEIR CONTRACTOR, THEIR SUCCESSORS OR ASSIGNS AS APPLICABLE) TO PURSUE ANY NEGOTIATIONS, OBTAIN ANY AGREEMENTS AND/OR PERMITS, ETC. FROM ALL NECESSARY OWNER, PRIVATE AND/OR GOVERNMENTAL AGENCIES IN CHARGE OF PROPERTIES AND/OR RIGHTS-OF-WAY ADJACENT TO (OR NEIGHBORING) THIS PROJECT, THAT MAY BE REQUIRED TO DO ANY WORK (CONSTRUCTION, ACCESS, MODIFICATIONS, GRADING, DRAINAGE, STRUCTURES, ROADS, ETC.) ENCRDACHING OR AFFECTING - DIRECTLY OR INDIRECTLY- ON THESE ADJACENT PROPERTIES AND RIGHTS-OF-WAY IN ANY CONCEIVABLE MANNER, REGARDLESS OF WHETHER OR NOT THIS WORK IS SHOWN OR DESCRIBED ON THESE PLANS.

ADDITIONAL GRADING GENERAL NOTES

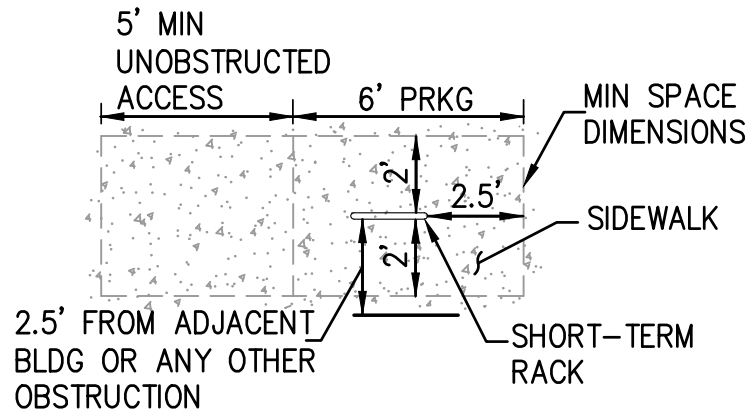
- THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY QUANTITIES AND BASE THEIR BID SOLELY ON THEIR OWN ESTIMATE.
- THE CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES PRIOR TO ANY CONSTRUCTION. CALL BLUE STAKE (1-800-782-5348) TWO (2) FULL WORKING DAYS PRIOR TO CONSTRUCTION. THE PLAN LOCATION OF UTILITIES SHOWN ARE APPROXIMATE, BASED ON AVAILABLE RECORDS AND SURVEY. NEITHER THE OWNER NOR THE ENGINEER CAN GUARANTEE THE ACCURACY OR COMPLETENESS OF UTILITIES SHOWN. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION AND/OR ELEVATION OF EXISTING UTILITIES WHICH PERTAIN TO AND AFFECT CONSTRUCTION OF THIS PROJECT.
- CUT OR FILL SLOPES 3:1 OR LESS STEEP MUST BE RE-VEGETATED IN ACCORDANCE WITH THE LANDSCAPE PLAN.
- THE ENGINEER SHALL NOT BE RESPONSIBLE OR LIABLE FOR UNAUTHORIZED CHANGES, OR USES OF THESE PLANS. ALL CHANGES TO THESE PLANS MUST BE APPROVED IN WRITING BY THE ENGINEER.
- THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING THE SAFETY OF ALL PERSONS AND PROPERTY. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE OWNER, ARCHITECT, AND ENGINEER HARMLESS FROM LIABILITY REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER, ARCHITECT, OR ENGINEER.
- GRADING SHALL NOT COMMENCE UNTIL A GRADING PERMIT HAS BEEN ISSUED BY THE TOWN OF ORO VALLEY.
- THE PORTION OF THE SITE TO BE GRADED SHALL BE CLEARLY MARKED PRIOR TO GRADING, CLEARING OR GRUBBING. CONSTRUCTION EQUIPMENT SHALL ONLY BE PERMITTED WITHIN MARKED AREA.
- ALL FILL AREAS SHALL BE COMPACTED TO A MINIMUM OF 95% MAX DENSITY. CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ANY COMPACTION TESTING RECORDS AND/OR CERTIFICATIONS REQUIRED BY THE TOWN OF ORO VALLEY. REFER TO GRADING PERMIT CONDITIONS.
- ALL ORGANIC MATERIAL SHALL BE REMOVED WITHIN THE CLEARING LIMITS FOR NECESSARY GRADING TO A DEPTH OF EIGHT (8) INCHES AND HAULED FROM THE SITE PRIOR TO GRADING.
- THE SOILS ENGINEER SHALL OBSERVE, INSPECT AND TEST ALL EARTHWORK OPERATIONS, INCLUDING BUT NOT LIMITED TO CLEARING, GRUBBING, SUBGRADE PREPARATIONS, STRUCTURAL, AND TRENCH EXCAVATION AND BACKFILL TOGETHER WITH THE PLACEMENT AND COMPACTION OF FILL.
- ALL CUT AND FILL SLOPES ARE TO BE REVEGETATED & ALL SLOPES TO BE A MAXIMUM OF 3:1, UNLESS OTHERWISE NOTED.
- ALL WATER VALVE BOXES, STORM DRAIN, SEWER AND OTHER UTILITY MANHOLES WITHIN THE AREAS OF GRADING OR PAVING SHALL BE BROUGHT FLUSH TO FINISH GRADE.
- WHEN MATCHING EXISTING PAVEMENT OR CURB AND GUTTER THE CONTRACTOR MUST VERIFY EXISTING ELEVATIONS TO ENSURE PROPER DRAINAGE AND GRADES. ANY DISCREPANCIES WITH THE PLANS SHALL BE IMMEDIATELY FORWARDED TO THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE FOR DUST CONTROL AT ALL TIMES DURING SITE GRADING.
- ANY ENGINEERING WORK TO BE DONE BELOW GRADE (I.E. TOE-DOWNS, CUTOFF WALLS, DRAINAGE PIPES/STRUCTURES, ETC.) SHALL NOT BE BACK FILLED UNTIL THE TOWN OF ORO VALLEY INSPECTOR INSPECTS THE WORK AND APPROVES IT.

PAVING NOTES

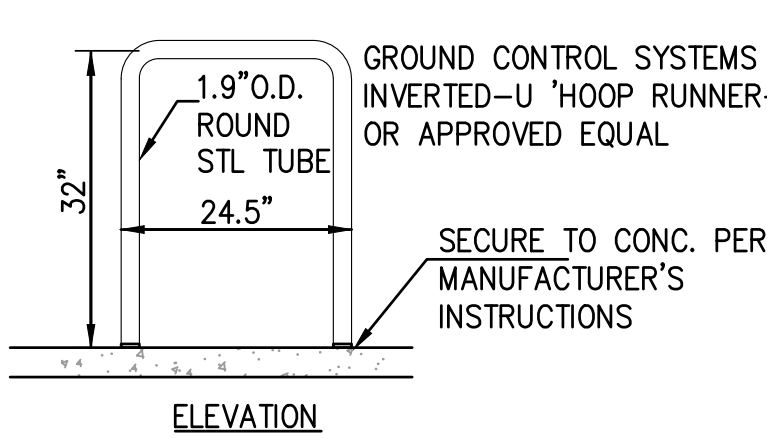
- THE PAVEMENT SECTIONS SHALL BE AS NOTED ON THE PLAN.
- ASPHALTIC CONCRETE SHALL CONFORM TO PAG SSPI SECTION 406, MIX# 2
- AGGREGATE BASE COURSE SHALL CONFORM TO PAG SSPI SECTION 303.
- ALL EXCAVATION/PAVING IS TO PROVIDE FOR DRAINAGE AS INDICATED BY FLOW ARROWS.

ACCESSIBLE PATH REQUIREMENTS

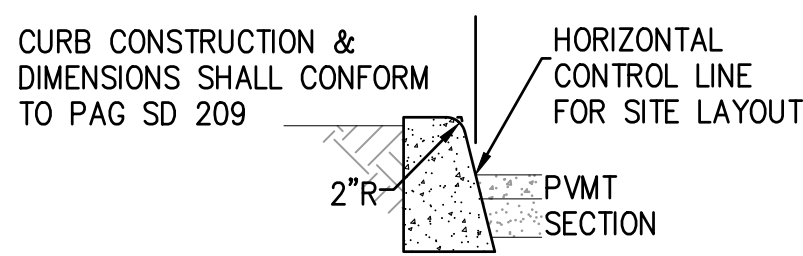
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL NEW ACCESSIBLE PATHS MEET THE DIMENSIONAL AND SLOPE REQUIREMENTS FOR SIDEWALKS, SIDEWALK RAMPS, CURB ACCESS RAMPS, LANDINGS, CROSSWALKS, HANDRAILS, DETECTABLE WARNINGS, HANDICAP PARKING SPACES AND ACCESS AISLES. REFER TO ICC/ANSI A117.1.
MAXIMUM LONGITUDINAL SLOPE (NON-RAMP)=5%
MAXIMUM CROSS SLOPE=2%
MAXIMUM RAMP SLOPE=12:1 (8.33%)
MAXIMUM RAMP RISE BEFORE HANDRAIL IS REQUIRED=6"
MINIMUM LANDING LENGTH=5', MINIMUM WIDTH MUST MATCH APPROACH SIDEWALK/RAMP WIDTH
REFER TO ICC/ANSI A117.1 FOR MORE DETAILED REQUIREMENTS.



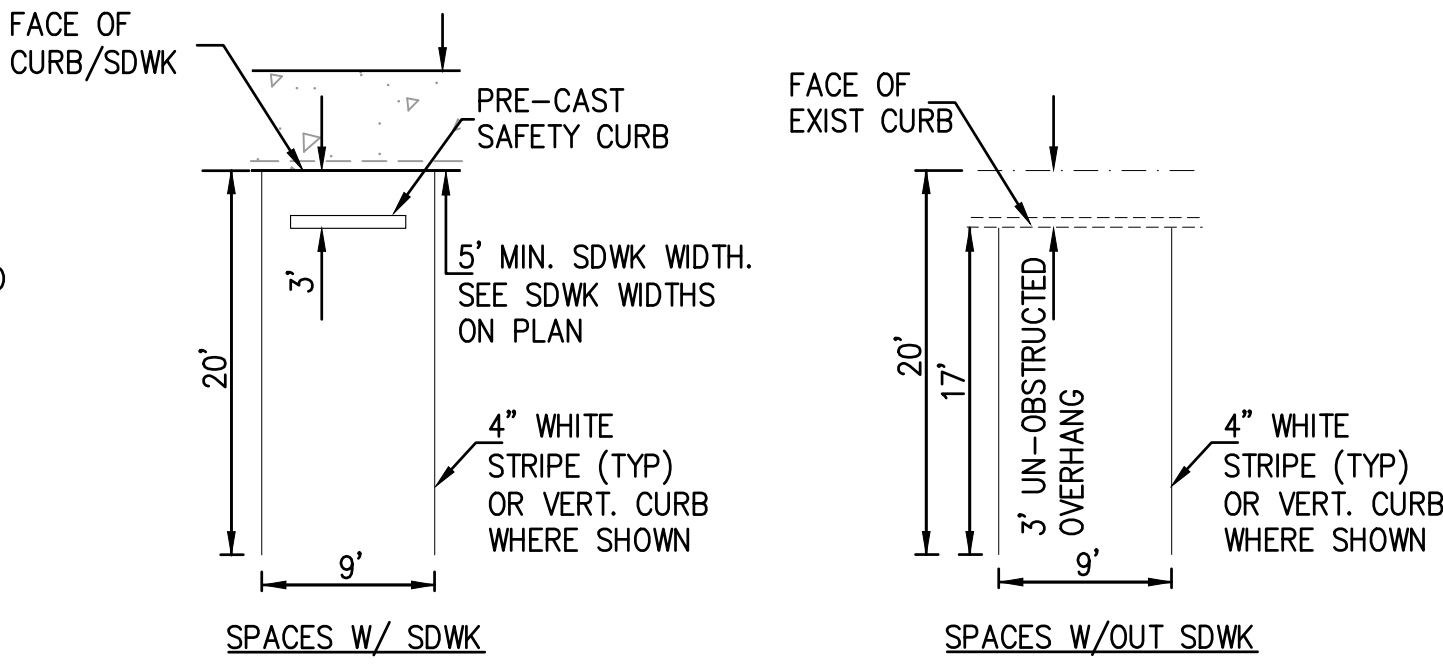
TYPICAL PLAN VIEW



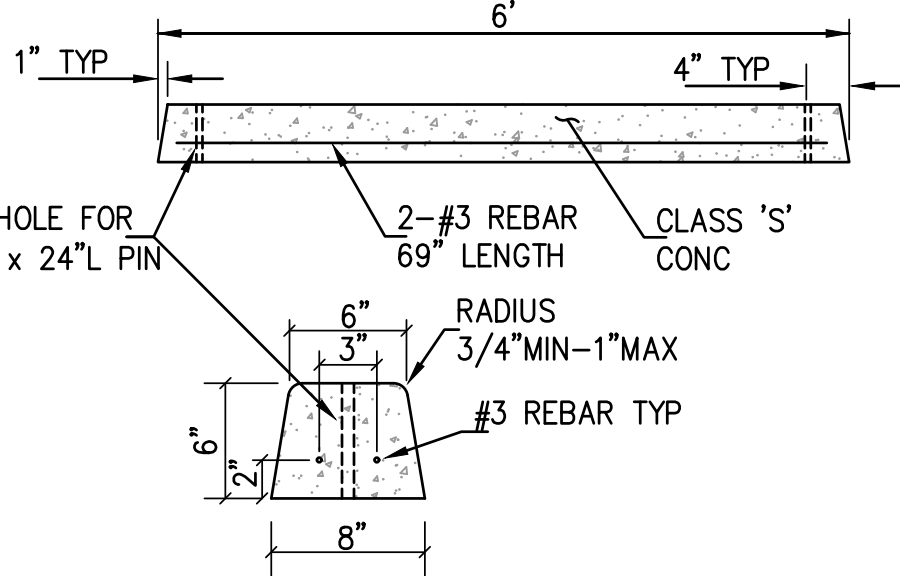
CLASS II BIKE PARKING



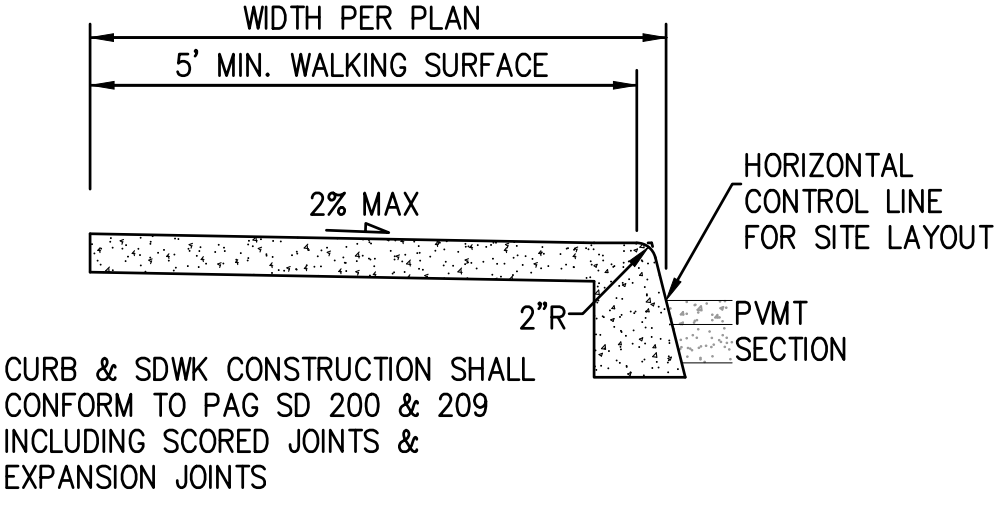
SURVEY CONTROL-
TYPE 2 VERT. CURB



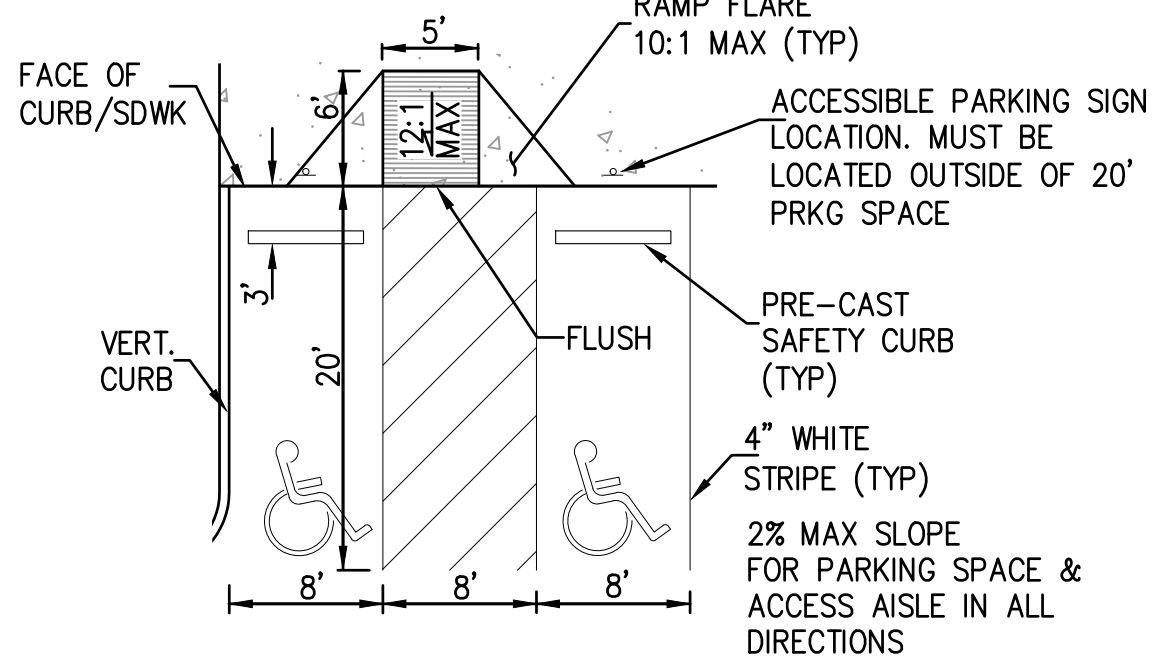
TYPICAL PARKING SPACES



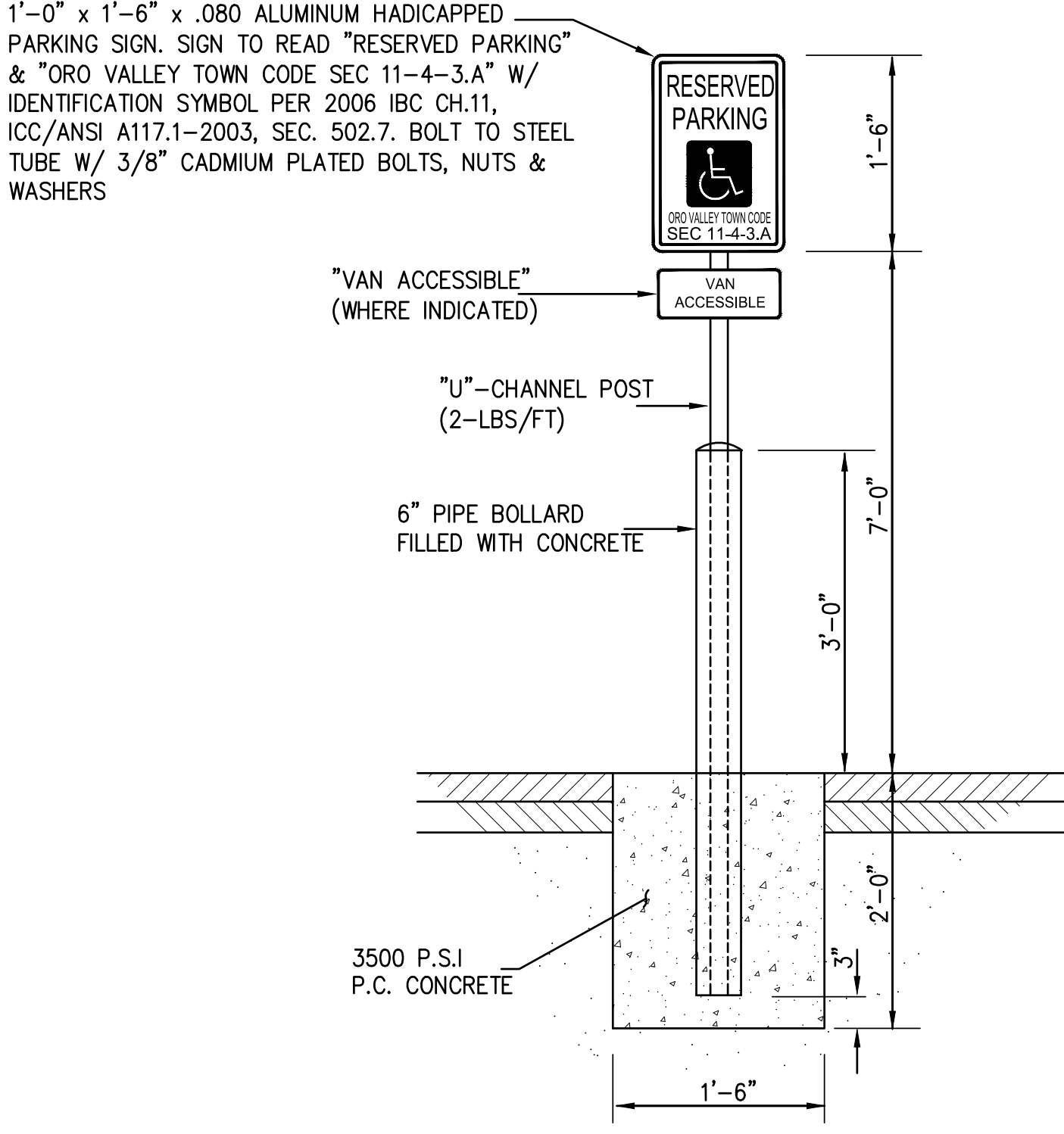
PRE-CAST SAFETY CURB



MONOLITHIC CURB/SIDEWALKS



VAN-ACCESSIBLE PARKING



ACCESSIBLE PARKING SIGN



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CONCEPTUAL SITE PLAN
TROPICAL SMOOTHIE-LA CANADA DR

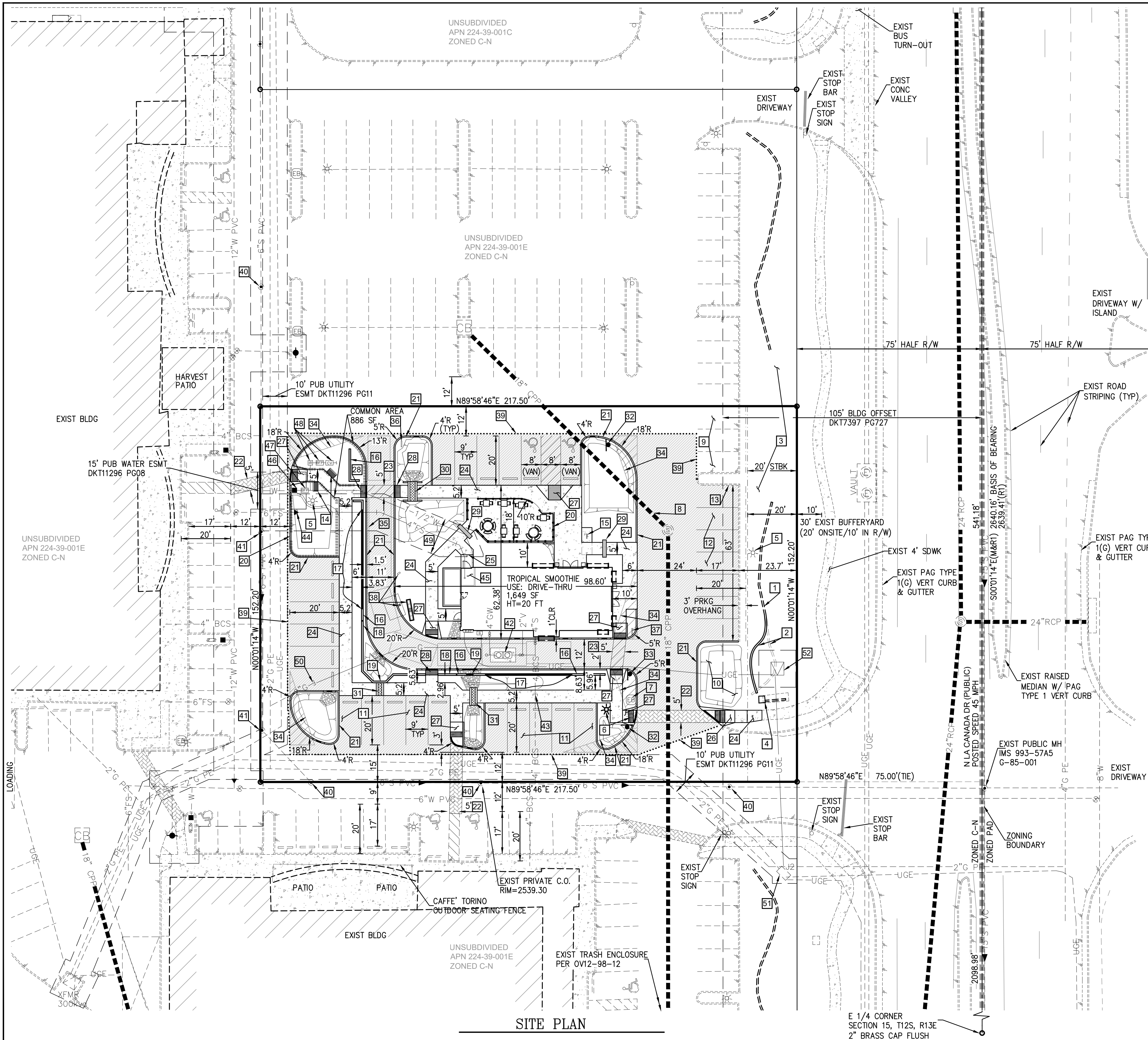
LOCATED IN THE N.E. 1/4 OF SEC 15, T12S, R13E, G&SRM,
TOWN OF ORO VALLEY, PIMA COUNTY, ARIZONA

CASE NO:		2202232	
JOB NO. D21017	SCALE HORIZ: N/A	SHEET 2 OF 5	
DATE: JULY, 2023	CONTOUR INTERVAL=N/A		



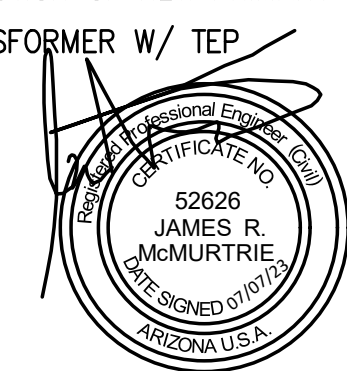
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REF. NO: OV12-98-12
OV10-97-9
OV9-96-7
PRE-APP 2102451
CUP 2202252



SITE PLAN KEYNOTES

- EXIST SCREEN WALL TO REMAIN
- RELOCATE PORTION EXIST SCREEN WALL TO ACCOMMODATE NEW ELEC. XFMR
- EXIST BERM & VEGETATIVE SCREEN TO REMAIN
- EXIST CANADA CROSSROADS MONUMENT SIGN (MASONRY COLUMN W/ ATTACHED LIGHTED BOX) TO REMAIN
- EXIST LIGHT POLE TO REMAIN
- EXIST LIGHT POLE. RELOCATE ±10.4' NORTHWEST TO LOCATION SHOWN.
- EXIST ELEC. PULL BOX TO REMAIN
- EXIST 18" STORM DRAIN TO REMAIN
- EXIST PARKING TO REMAIN
- REMOVE 2 EXIST PARKING SPACES. SEE LANDSCAPE PLAN
- RECONFIGURE 14 EXIST PARKING SPACES TO 10 SPACES W/ ISLANDS AS SHOWN
- RE-GRADE, REPLACE PAVING & RE-STRIPE 7 EXIST PARKING SPACES
- EXIST LANDSCAPE ISLAND TO REMAIN
- COMMON AREA SEATING BENCH
- CLASS II BIKE RACK LOCATION, 1 RACK (2 BIKES). REFER TO DETAIL ON SHEET 2
- 3.5' HIGH OPAQUE SCREEN WALL W/ TRELLIS ABOVE WHERE INDICATED
- TRELLIS EXTENSION ABOVE SCREEN WALL FOR EXTRA SCREENING. TRELLIS TO EXTEND 3' ABOVE WALL. 40' LONG AT WEST WALL. 35' LONG AT DRIVE-THRU WINDOW. LOCATIONS AS SHOWN.
- 4.5' HIGH OPAQUE SCREEN WALL AROUND CURVED PORTION OF DRIVE-THRU
- OMIT 2 CMUs (8"H X 2.67"W) FOR DRAINAGE OPENING AT BOTTOM OF WALL GRADE PER GRADING PLAN
- 3' BARRIER FENCE AROUND OUTDOOR SEATING AREA
- VERT CURB PER PAG SD 209. AT EXIST BLDG PAD PERIMETER, EXTRUDED CURB SHALL BE REMOVED & REPLACED W/ VERT CURB
- HERRINGBONE BRICK PATTERN-STAMPED CLAY-RED CONC. CROSSWALK. MATCH EXISTING
- STRIPED CROSSWALK. 4" WHITE STRIPES
- SDWK PER PAG SD 200. SDWKs, WHERE ADJ. VERT. CURB, MAY BE POURED MONOLITHICALLY W/ THE CURB PROVIDED ALL SDWK STANDARDS PER PAG SD 200 ARE FOLLOWED. SEE DETAIL SHEET 2
- 6" STEP-UP ONTO LANDING
- RECONSTRUCT DAMAGED SDWK RAMP. REFER TO DETAIL ON SHEET 5
- SDWK RAMP. REFER TO DETAILS ON SHEET 5
- 2' TRUNCATED DOME DETECTABLE WARNING STRIP PER ICC/ANSI A117.1 EXTENDING FULL WIDTH OF SDWK
- TYPE 1 SDWK SCUPPER PER PAG SD 204. EXTEND 1' BEYOND EDGE OF SIDEWALK.
- TYPE 2 SDWK SCUPPER PER PAG SD 205. OPEN WIDTH 'A'=2'. OPEN HEIGHT 'H'=0.5'.
- TYPE 2 SDWK SCUPPER PER PAG SD 205. OPEN WIDTH 'A'=1.33'. OPEN HEIGHT 'H'=0.5'. WHERE ABUTTING WALL, OMIT 1 CMU AT BOTTOM OF WALL FOR DRAINAGE OPENING AT SCUPPER INVERT ELEV.
- RELOCATE & REPLACE EXIST STOP SIGN
- NEW STOP SIGN
- 10'x30' PAAL-TO-PAAL SVT
- 9'x18' DRIVE-THRU STAKING SPACE. TYPICAL OF 8 SPACES
- DRIVE-THRU ENTRY SIGN BY OTHERS
- DRIVE-THRU EXIT-ONLY SIGN BY OTHERS
- DRIVE-THRU SPEAKER, MENU & RELATED EQUIPMENT
- SAWCUT, TACK & JOIN TO EXIST ASPHALT PMVT
- EXIST PRIVATE SEWER CLEANOUT (VISIBLE) - 6" PVC SEWER
- EXIST PRIVATE SEWER CLEANOUT (NOT VISIBLE) - 6" PVC SEWER
- GREASE INTERCEPTOR
- LOCATE & CONNECT TO EXIST 4" PRIVATE BUILDING SEWER STUB-OUT. LOCATION UNKNOWN & DRAWN BASED ONLY ON DEVELOPMENT PLAN OV12-98-12 (BK19, PG30)
- EXIST 6" FIRE SERVICE STUB-OUT. LOCATE/CONNECT W/ 4" REDUCER. ROUTE 4" DIP FIRE SERVICE, RESTRAINED FULL LENGTH. SEPARATE WATER IMPROVEMENT PLANS REQUIRED. GOLDER RANCH FIRE TO CONDUCT INSPECTIONS. OVWU TO CONDUCT INSPECTIONS.
- FIRE RISER ROOM W/ EXTERIOR DOOR. RISER TO ACCOMMODATE USC CERTIFIED DOUBLE-CHECK BACKFLOW PREVENTER PER OVWU STANDARDS. GOLDER RANCH FIRE TO CONDUCT INSPECTIONS. OVWU TO CONDUCT INSPECTIONS.
- EXIST 2" WATER SERVICE W/ EXIST METER BOX. COORDINATE INSTALLATION OF 1" METER & PRIVATE SHUT-OFF VALVE W/ OVWU. THE HIGH PUBLIC WATER SYSTEM PRESSURE REQUIRES THE INSTALLATION OF A PRESSURE REGULATOR ON THE BUILDING PLUMBING. THE INSTALLATION OF A PRESSURE REGULATOR MAY CREATE A CLOSED SYSTEM. CONSULT LOCAL PLUMBING CODES FOR PRESSURE RELIEF AND THERMAL EXPANSION REQUIREMENTS. REFER TO PLUMBING PLANS
- INSTALL 1" RPZ BACKFLOW PREVENTER PER OVWU STANDARDS. PROVIDE INSULATED COVER W/ METAL CAGE & LOCK
- INSTALL PRIVATE 5/8" IRRIGATION SUB-METER & BACKFLOW PREVENTER. PROVIDE INSULATED COVER W/ METAL CAGE & LOCK. INSTALL IRRIGATION CONTROL VALVE PER IRRIGATION PLANS.
- PROPOSED 15' PUBLIC WATERLINE EASEMENT TO BY SEPARATE INSTRUMENT TO BE GRANTED FOLLOWING WATERLINE CONSTRUCTION.
- LOCATE GAS STUB-OUT FOR CONNECTION. COORDINATE W/ SWG. ROUTE GAS TO BLDG PER PLUMBING CODE REQMTs
- EXIST TEP 'J2'. COORDINATE CONNECTION OF NEW PRIMARY W/ TEP
- COORDINATE INSTALL OF NEW TRANSFORMER W/ TEP



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LOCATED IN THE N.E. 1/4 OF SEC 15, T12S, R13E, G&SRM,
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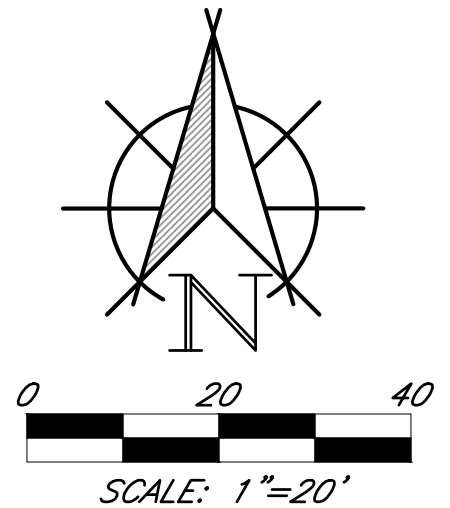
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SHEET 3 OF 5

ALL DIMENSIONS ARE TO CURB CONTROL LINE OR TO FACE OF WALL



NO.	BY	DATE	DESCRIPTION	APPROVED BY	DATE



GRADING & DRAINAGE KEYNOTES

- 1 HARVESTING DEPRESSION. 4"DEPTH, VOL=96CF
- 2 HARVESTING DEPRESSION. 4"DEPTH, VOL=35CF
- 3 HARVESTING DEPRESSION. 4"DEPTH, VOL=138CF
- 4 HARVESTING DEPRESSION. 4"DEPTH, VOL=60CF
- 5 HARVESTING DEPRESSION. 4"DEPTH, VOL=15CF
- 6 HARVESTING DEPRESSION. 4"DEPTH, VOL=52CF
- 7 HARVESTING DEPRESSION. 4"DEPTH, VOL=11CF
- 8 HARVESTING DEPRESSION. 4"DEPTH, VOL=16CF
- 9 HARVESTING DEPRESSION. 4"DEPTH, VOL=40CF
- 10 HARVESTING DEPRESSION. 4"DEPTH, VOL=106CF
- 11 2' CURB OPENING
- 12 6' CURB OPENING
- 13 OMIT 2 CMUs (8"H x 2.67"W) FOR DRAINAGE OPENING AT BOTTOM OF WALL
- 14 TYPE 1 SDWK SCUPPER PER PAG SD 204 FOR MINOR DRAINAGE. EXTEND 1' BEYOND EDGE OF SIDEWALK.
- 15 TYPE 2 SDWK SCUPPER PER PAG SD 205. OPEN WIDTH 'A'=2'. OPEN HEIGHT 'H'=0.5'.
- 16 TYPE 2 SDWK SCUPPER PER PAG SD 205. OPEN WIDTH 'A'=1.33'. OPEN HEIGHT 'H'=0.5'. WHERE ABUTTING WALL, OMIT 1 CMU AT BOTTOM OF WALL FOR DRAINAGE OPENING AT SCUPPER INVERT ELEV.
- 17 GROUTED RIP-RAP SWALE @ 3% SLOPE FROM ROOF DRAIN TO CURB OPENING. (ONLY 1 ROOF DRAIN FOR THE BLDG)
- 18 6" STEP-UP ONTO LANDING PER GRADES PROVIDED
- 19 REFER TO SDWK RAMPS DETAIL 1 ON SHEET 5
- 20 REFER TO SDWK RAMPS DETAIL 2 ON SHEET 5
- 21 REFER TO SDWK RAMPS DETAIL 3 ON SHEET 5
- 22 REFER TO SDWK RAMPS DETAIL 4 ON SHEET 5
- 23 REFER TO SDWK RAMPS DETAIL 5 ON SHEET 5
- 24 REFER TO SDWK RAMPS DETAIL 6 ON SHEET 5
- 25 REFER TO SDWK RAMPS DETAIL 7 ON SHEET 5
- 26 SEATING AREA SLAB FLUSH TO ADJ. SDWK ALONG NORTH EDGE
- 27 SEATING AREA SLAB GRADES & ADJ. SDWK GRADES DIVERGE ALONG EAST EDGE TO SHOW A SDWK REVEAL. PER GRADES PROVIDED. APPROPRIATE TURN-DOWN WILL BE DETAILED ON FINAL IMPROVEMENT PLANS
- 28 SEE DRIVE-THRU AREA GRADING DETAIL ON SHEET 5.

* WSELs BASED ON CANADA CROSSROADS DEVELOPMENT PLAN OV12-98-12 & ASSOCIATED DRAINAGE REPORT, WHICH BOTH UTILIZED AN ELEVATION DATUM WITH ELEVATIONS $\pm 1.5'$ LOWER THAN NAVD 88. WSEL IN PONDING AREAS ALSO ADJUSTED TO REFLECT EXISTING HIGH POINTS ELEVATIONS IN THE PARKING LOT.

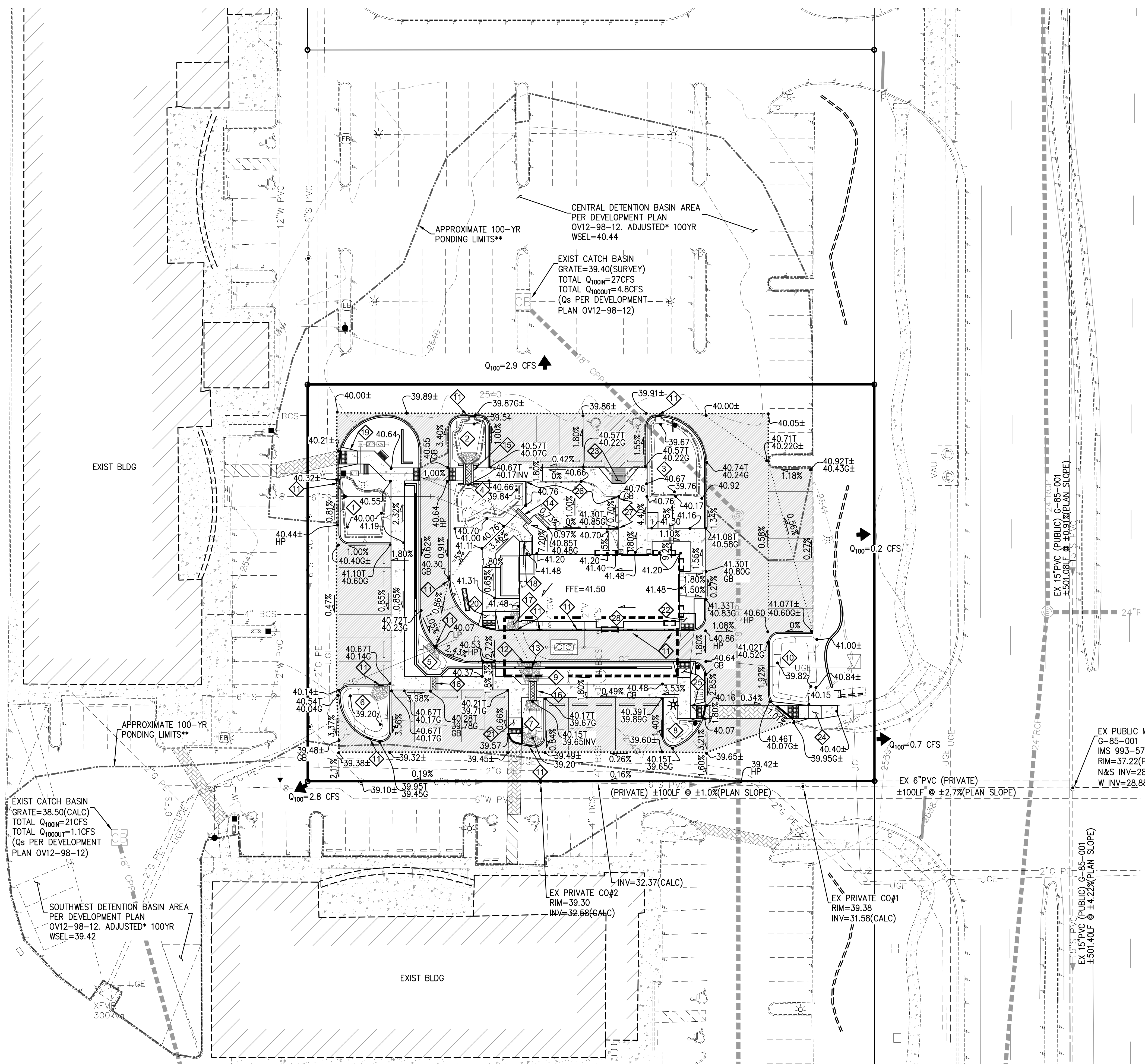
** THE PONDING LIMITS SHOWN ARE APPROXIMATE ONLY AND BASED ON COMPARISON OF THE ADJUSTED OV12-98-12 WSELs TO EXISTING SURVEY AND PAG 2015 ELEVATIONS & PROPOSED GRADES. NO RE-EVALUATION OF THE OVERALL CANADA CROSSROADS DRAINAGE FLOW RATES OR FLOOD-ROUTING OF THE BASINS HAVE BEEN PERFORMED.



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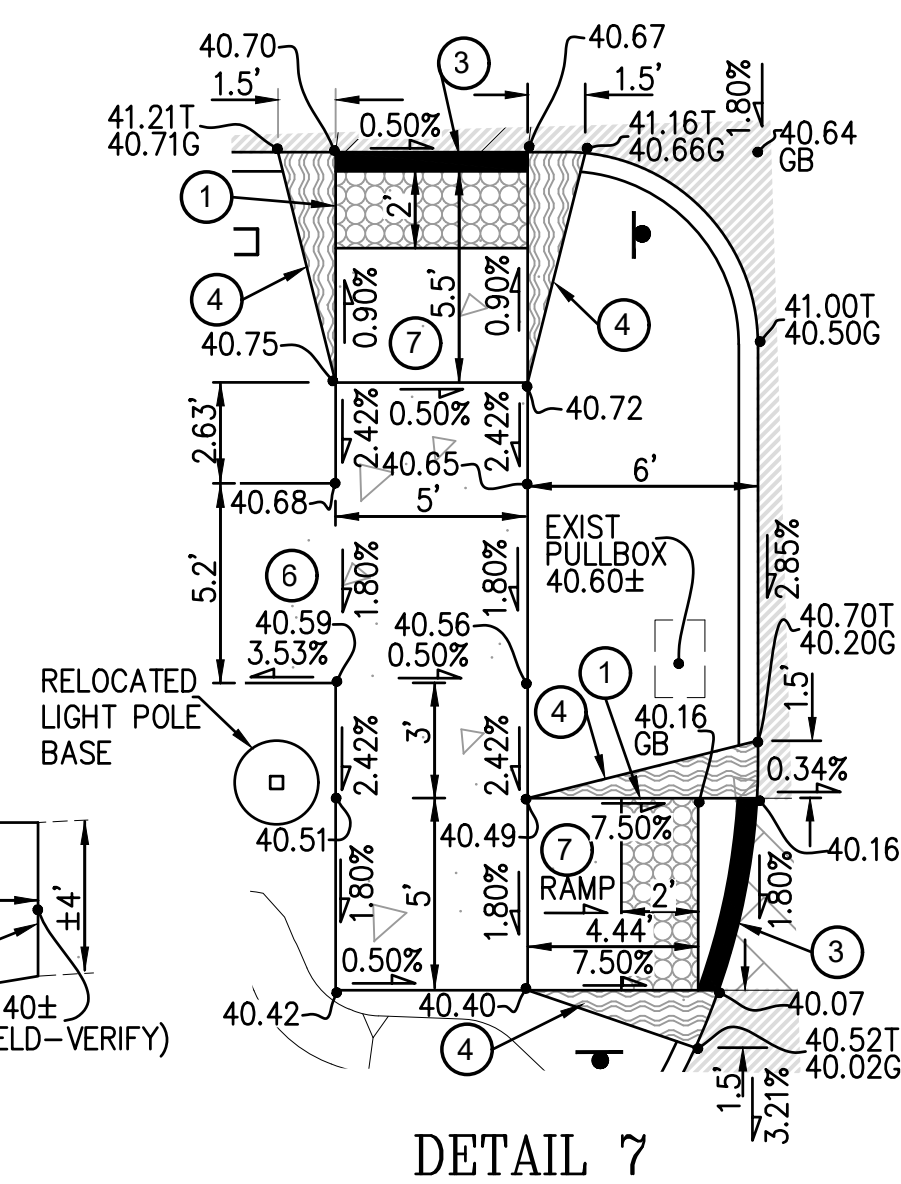
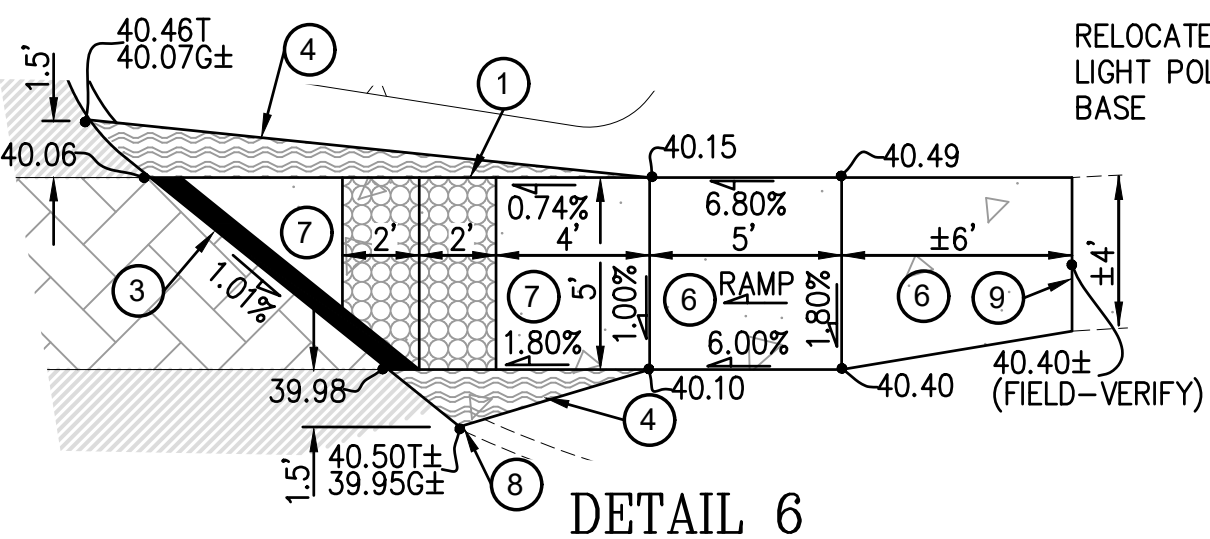
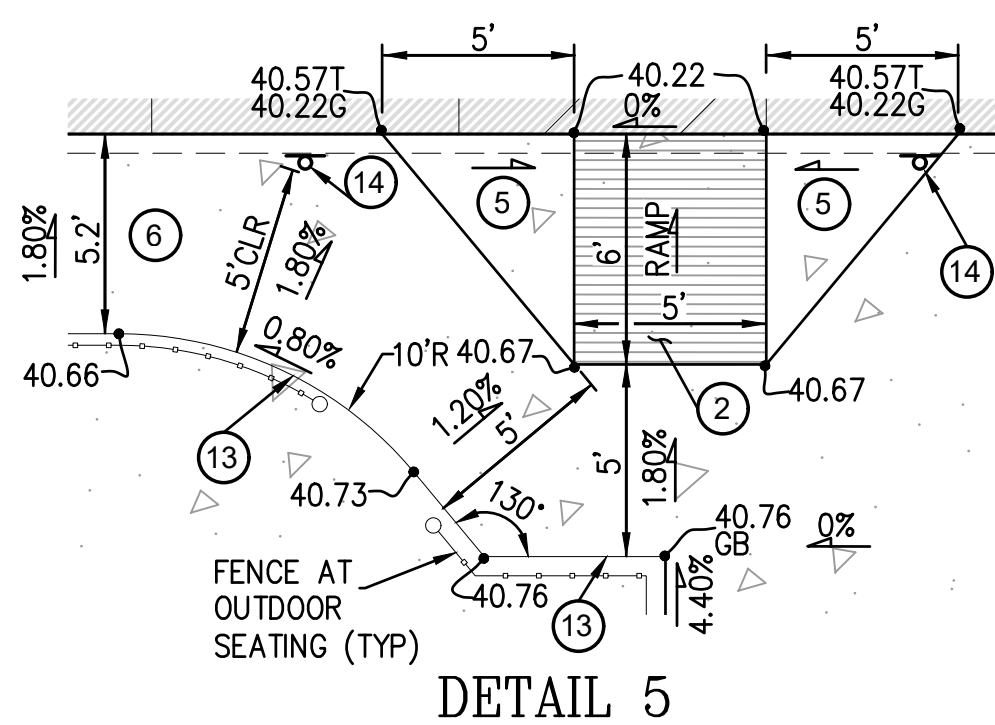
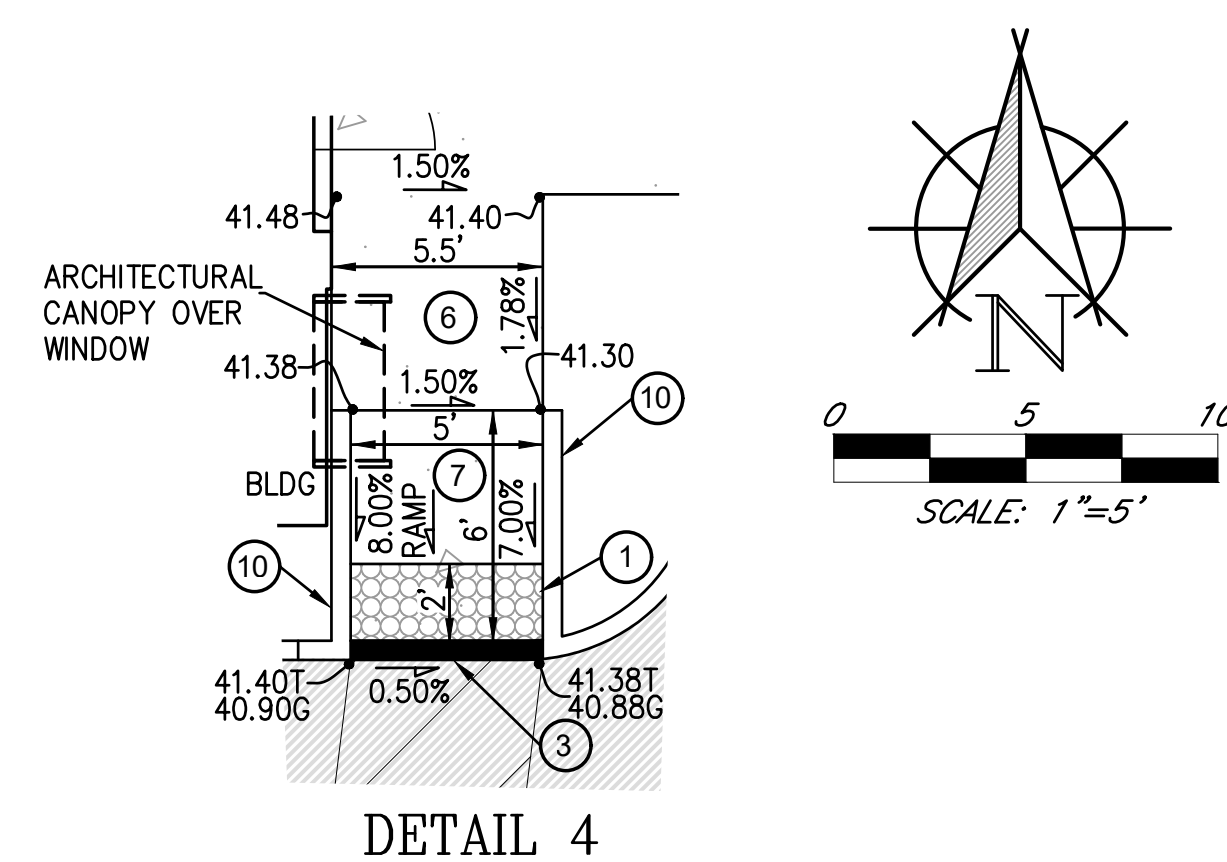
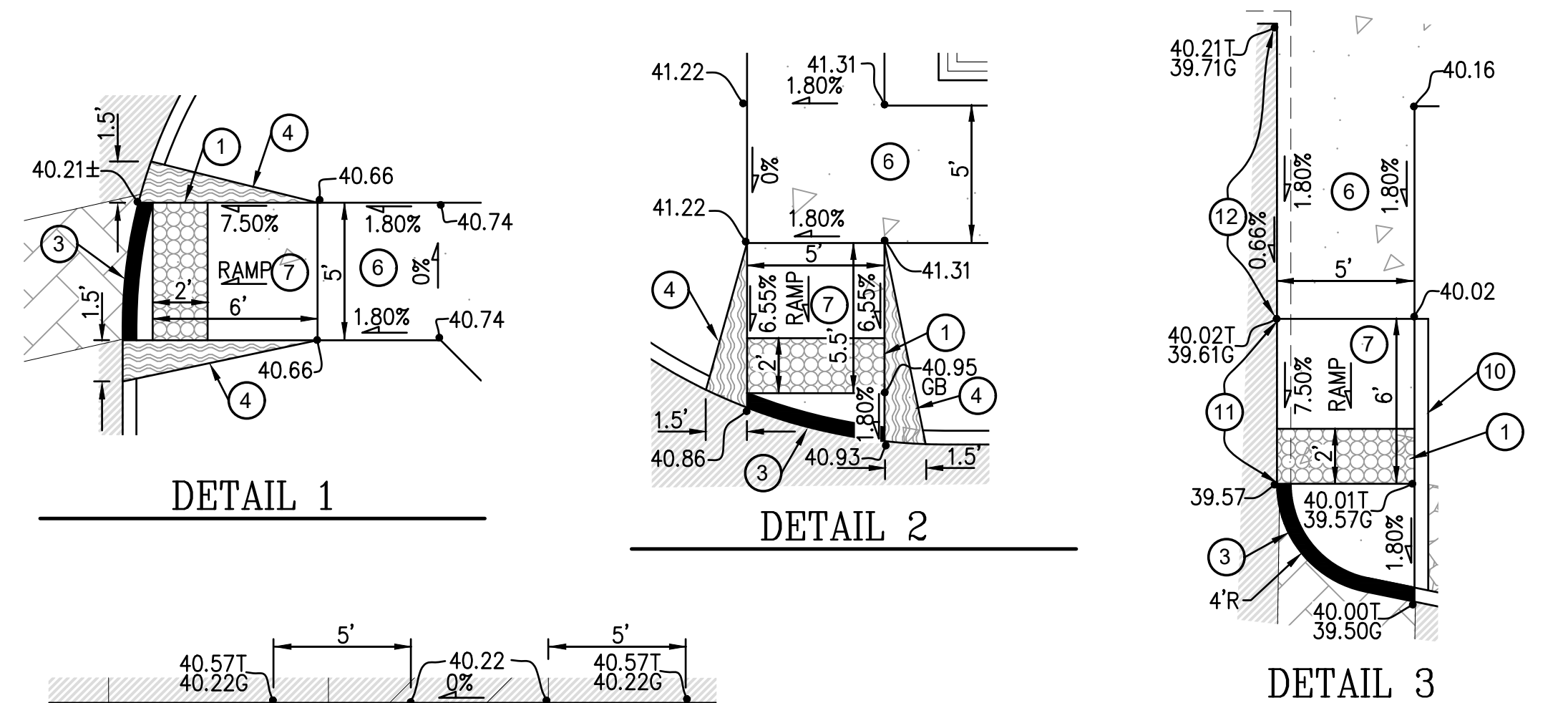
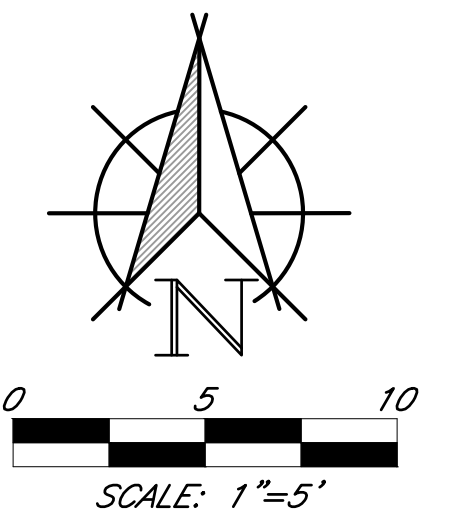
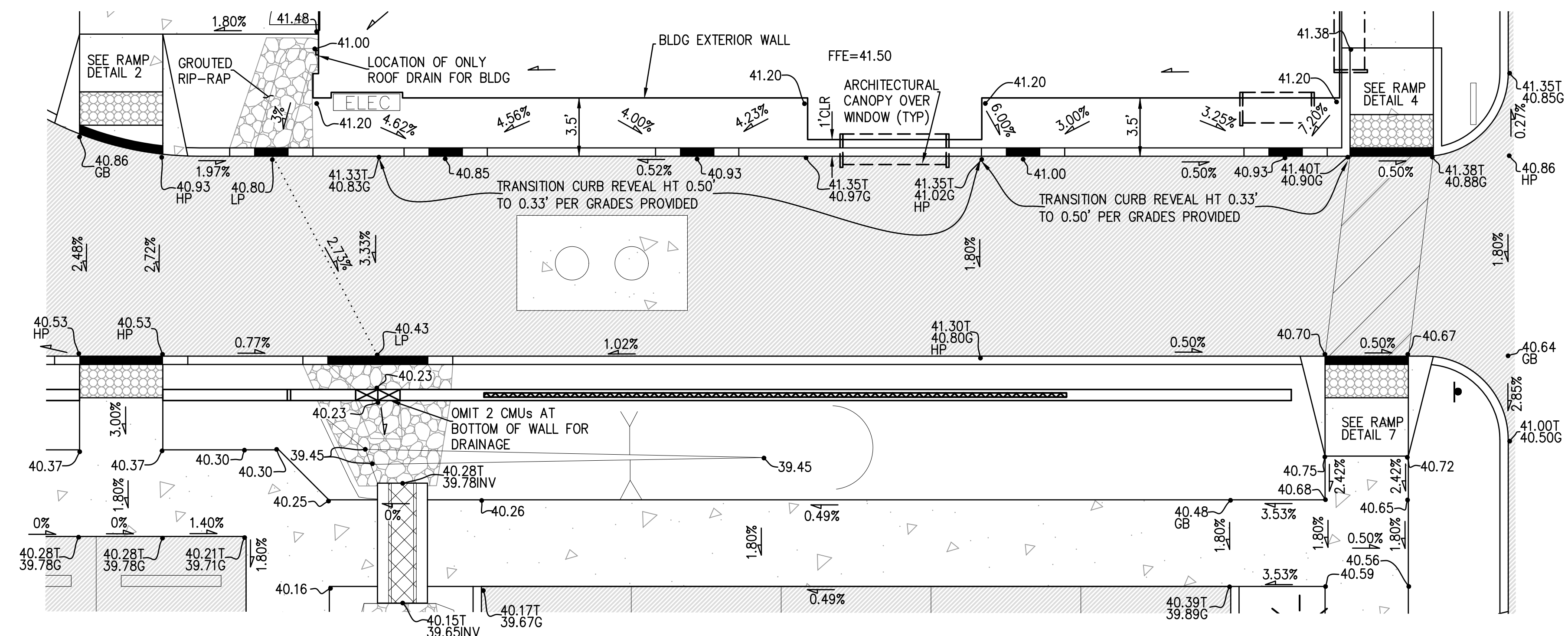
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SHEET 4 OF 5	



CONCEPT GRADING & DRAINAGE



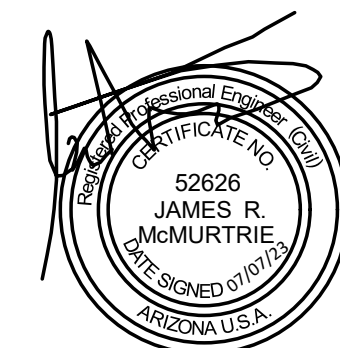
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DETAIL KEYNOTES ○

- ① TRUNCATED DOME DETECTABLE WARNING STRIP COMPLIANT W/ ICC/ASNI A117.1 SECTION 705. MATCH EXIST CLAY-RED WARNINGS AT NORTH SITE ENTRANCES
- ② FINISH RAMP SURFACE W/ TACTILE GROOVES MATCH EXIST RAMPS THROUGHOUT SHOPPING CENTER
- ③ DEPRESSED CURB FLUSH TO ADJ. PVMT OR CONC.
- ④ RAMP SIDE FLARE, 6" T CLASS 'B' CONC. W/ ROUGH BROOM FINISH ON FLARE ONLY. SLOPE TO MATCH INTO TOP OF CURB
- ⑤ RAMP SIDE FLARE 10:1 MAX PER ICC/ASNI A117.1 SECTION 406.
- ⑥ SDWK PER PAG SD 200
- ⑦ SDWK RAMP/LANDING SHALL BE 6" T CLASS 'B' CONC. RAMPS SHALL CONFORM TO ICC/ASNI A117.1. MAX 12:1 RAMP SLOPE. MAX 2% CROSS-SLOPE.
- ⑧ JOIN TO EXIST VERT. CURB PER PAG SD 211
- ⑨ JOIN TO EXIST SDWK PER PAG SD 203
- ⑩ CONTINUE VERT. CURB ALONG SIDE OF RAMP. TOP OF CURB TO MATCH SDWK GRADE AT TOP OF RAMP
- ⑪ TRANSITION VERT. CURB REVEAL 0' TO 0.41'
- ⑫ TRANSITION VERT. CURB REVEAL 0.41' TO 0.50'
- ⑬ SEATING AREA SLAB FLUSH TO ADJ. SDWK ALONG NORTH EDGE SHOWN IN DETAIL 5
- ⑭ ACCESSIBLE PRKG SIGN PER DETAIL

SDWK RAMP DETAILS



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